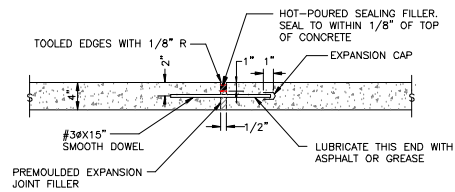
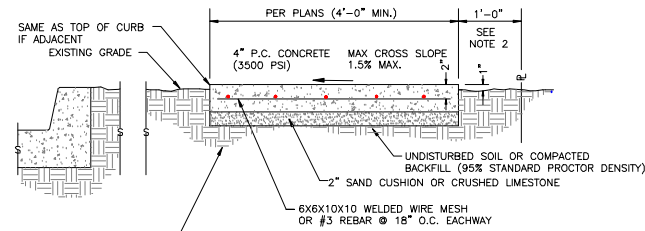


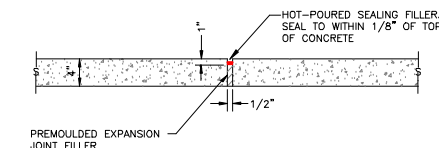
SIDEWALK PLAN AND JOINT LAYOUT



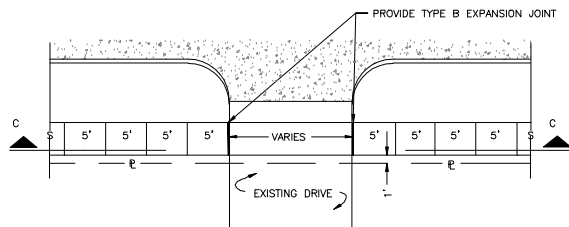
TYPE A TRANSVERSE EXPANSION JOINT



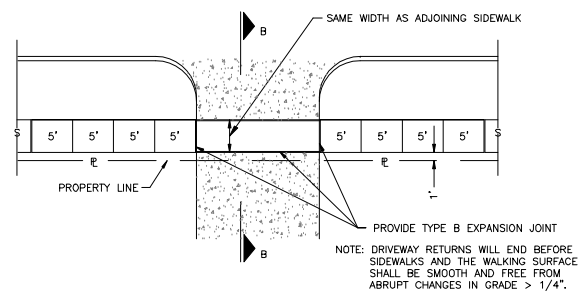
SECTION A-A



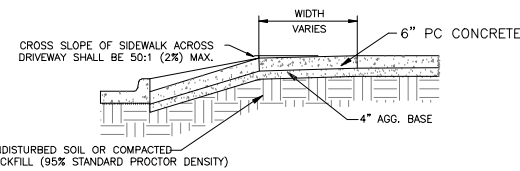
TYPE B EXPANSION JOINT



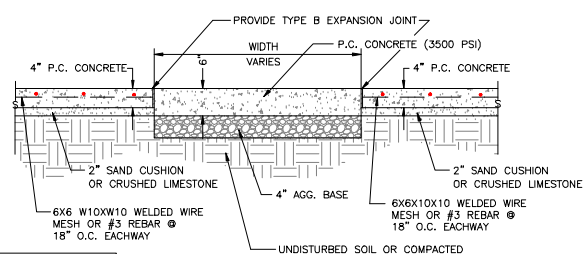
SIDEWALK AT EXISTING DRIVE OTHER THAN CONCRETE



SIDEWALK AT CONCRETE DRIVE

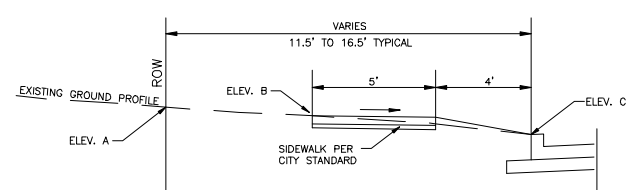


SECTION B-B



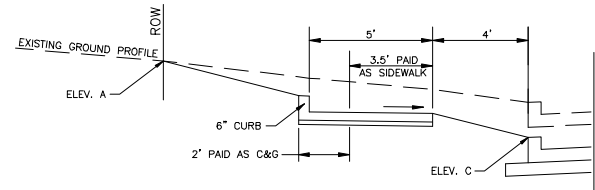
SECTION C-C

NOTE: LONGITUDINAL SLOPE OF SIDEWALK SHALL NOT EXCEED 5%



SECTION A

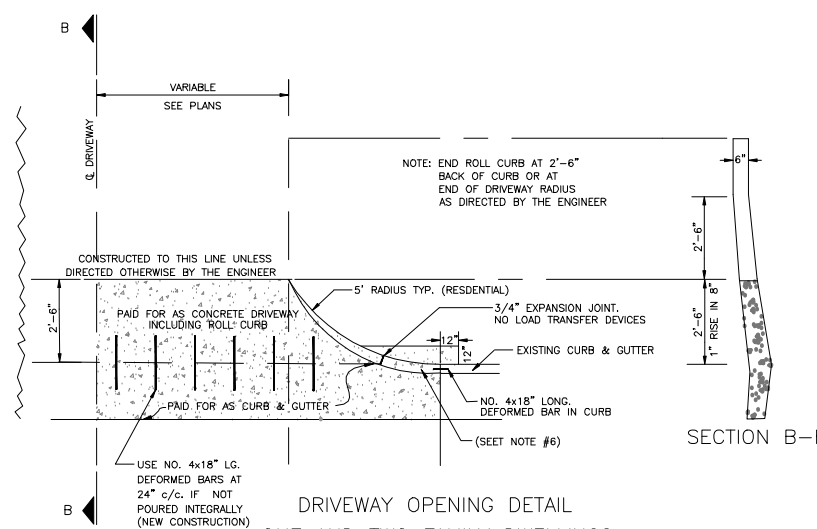
WHEN (ELEV. B - ELEV. C) IS LESS THAN 7" OR WHEN (ELEV. A - ELEV. C) IS LESS THAN 1.5" (NOTE: NEAR EDGE OF SIDEWALK WILL BE 6" ABOVE ELEV. C)



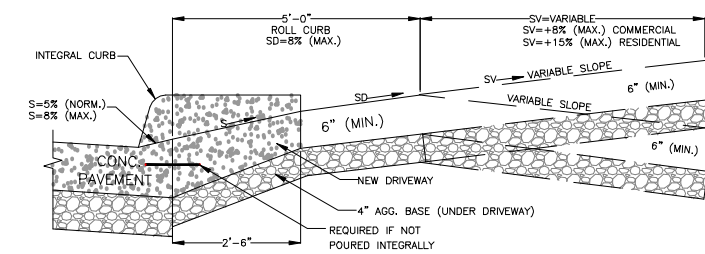
SECTION B

WHEN (ELEV. A - ELEV. C) IS GREATER THAN 1.5" (NOTE: NEAR EDGE OF SIDEWALK WILL BE 6" TO 10" ABOVE ELEV. C)

SIDEWALK PLACEMENT DETAILS (FOR USE AT LOCATIONS AS DIRECTED BY CITY ENGINEER)



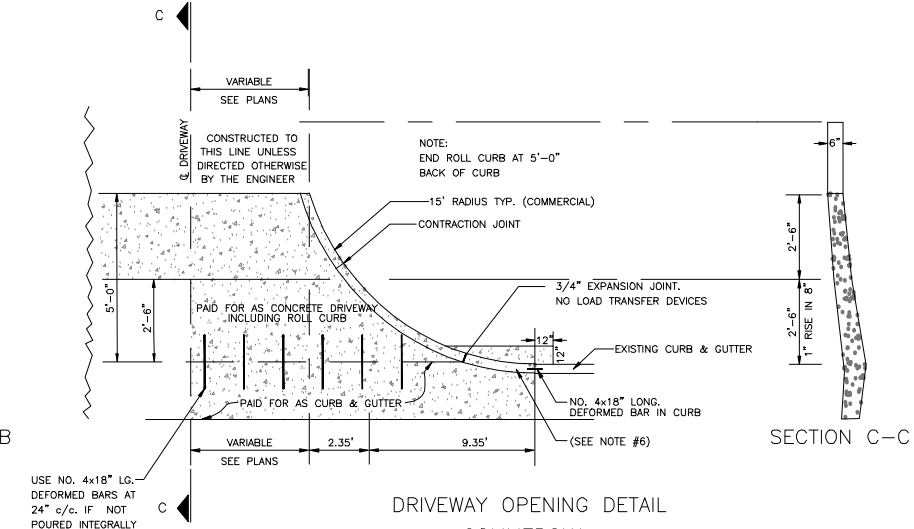
DRIVEWAY OPENING DETAIL ONE AND TWO FAMILY DWELLINGS



DRIVEWAY OPENING DETAIL ALONG CENTERLINE OF DRIVE (NOT TO SCALE)

- MINIMUM SIDEWALK WIDTHS SHALL BE DETERMINED AS FOLLOWS UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER.
  - \*ALONG ARTERIALS 6'-0"
  - \*ALONG COLLECTORS 5'-0"
  - \*WITHIN TWO BLOCKS OF A SCHOOL 5'-0"
  - \*R-3 & R-4 ZONING 5'-0"
  - \*ALL OTHER 4'-0"
- PUBLIC SIDEWALKS SHALL BE INSTALLED IN THE RIGHT-OF-WAY WITHIN 1' OF THE PROPERTY LINE UNLESS DIRECTED OTHERWISE BY THE CITY ENGINEER.
- METER BOXES, FIRE HYDRANTS, LIGHT POLES, SIGN POSTS, ETC., WILL NOT BE INSTALLED WITHIN ANY PORTION OF THE SIDEWALK. VALVE BOXES INSTALLED IN SIDEWALKS SHALL BE CONSTRUCTED SO THAT PORTION OF THE SIDEWALK MEETS REQUIRED ADA SPECIFICATIONS.
- THERE WILL BE A MAX. DROP ACROSS SIDEWALKS FOR DRAINAGE AS FOLLOWS:
 

SIDEWALK WIDTH	DROP ACROSS SIDEWALK
4'	3/4"
5'	1"
6'	1 1/4"
8'	1 1/2"
10'	1 3/4"
OTHER	1.5% MAX.
- DRIVEWAY RETURNS WILL NOT CROSS SIDEWALKS.
- EXPANSION JOINT EVERY 150' MIN.



DRIVEWAY OPENING DETAIL COMMERCIAL

DRIVEWAY CONSTRUCTION NOTES

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH CITY OF LAWTON STANDARD SPECIFICATIONS.
- WHERE CONCRETE PAVEMENT WITH INTEGRAL CURB IS IN PLACE: REMOVE 2' WIDE SECTION OF CURB & GUTTER TO A NEAT LINE USING FULL DEPTH SAW CUT AND CONSTRUCT NEW DRIVEWAY USING BUTT JOINT WITH #4x18" DEFORMED BARS DRILLED AT 24" c/c AND EPOXY GROUTED INTO EXISTING PAVEMENT.
- WHERE CONCRETE PAVEMENT OR ASPHALT PAVEMENT WITH A CURB & GUTTER SECTION IS IN PLACE: REMOVE EXISTING CURB & GUTTER THRU EXTENT OF PLANNED DRIVEWAY CONSTRUCTION TO A NEAT LINE USING FULL DEPTH SAW CUT. FOR CONCRETE PAVEMENT, CONSTRUCT NEW DRIVEWAY USING BUTT JOINT WITH #4x18" DEFORMED BARS DRILLED AT 24" c/c AND EPOXY GROUTED INTO EXISTING PAVEMENT. FOR ASPHALT PAVEMENT USE BUTT JOINT ONLY. NEW DRIVEWAY SHALL BE OF P.C. CONCRETE THRU EXTENT SHOW BY SHADING (MINIMUM).
- EXPANSION JOINT MATERIAL SHALL EXTEND TO WITHIN 1" OF PAVING OR CURB SURFACE AND THEN SHALL BE FILLED TO SURFACE WITH JOINT SEALER.
- WHERE DRIVEWAYS INTERSECT WITH SIDEWALKS, TRANSITIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ADA REQUIREMENTS.
- DRIVEWAY CURBING SHALL BE POURED INTEGRALLY WITH CURB AND GUTTER, BLOCK OUT OF GUTTER SHALL OCCUR IF NOT POURED MONOLITHIC.
- DRIVEWAY APPROACHES SHALL HAVE CONTROL JOINT TYPE 'c' AT MAXIMUM OF 15' O.C. EACHWAY.

SIDEWALK CONSTRUCTION NOTES

- ALL NEW SIDEWALKS WILL MEET ADA ACCESSIBILITY GUIDELINES AND AS SET FORTH IN ANSI A117.1 (2003) FOR ACCESSIBLE ROUTES:

(A) ANY PART OF AN ACCESSIBLE ROUTE WITH A SLOPE GREATER THAN 20:1 SHALL BE CONSTRUCTED TO ADA RAMP STANDARDS AS FOLLOWS AND WILL CONTAIN AT LEAST ONE LEVEL AREA 5'-0" IN LENGTH AT THE ENDS OF EACH RUN.

SLOPE	MAX. RISE	MAX. RUN
8:1 TO 9:1	3 IN (75 MM)	2 FT. (0.6M)
10:1 TO 11:1	6 IN (150 MM)	5 FT. (1.5M)
12:1 TO 15:1	30 IN (760 MM)	30 FT. (9M)
16:1 TO 19:1	30 IN (760 MM)	40 FT. (12M)
20:1	30 IN (760 MM)	50 FT. (15M)

- TRANSITIONS BETWEEN RAMPS, WALKS, GUTTERS, DRIVEWAYS, AND STREETS SHALL BE FLUSH AND FREE OF ABRUPT CHANGES. MAXIMUM SLOPES OF ADJOINING GUTTERS, ROAD SURFACE IMMEDIATELY ADJACENT TO THE CURB RAMP, OR ACCESSIBLE ROUTE SHALL NOT EXCEED 20:1.
- THE MINIMUM WIDTH OF A CURB RAMP SHALL BE 48" EXCLUSIVE OF CURBING, RETURNS, OR FLARED SIDES. IF THE RAMP IS A RETURNED CURB RAMP WITH CURBS, RAMP WIDTH SHALL BE THE SAME AS THE ADJOINING WALK WIDTH.
- IF A RAMP IS LOCATED WHERE PEDESTRIANS WILL WALK ACROSS THE RAMP, THEN IT SHALL HAVE FLARED SIDES. THE MAXIMUM SLOPE OF THE FLARE SHALL BE 12:1.
- ALL CURB RAMPS SHALL HAVE WIRE MESH OR EQUAL AND 2" SAND CUSHION OR CRUSHED LIMESTONE.

<b>STANDARD DETAILS SIDEWALK AND DRIVEWAY</b>			
<b>CITY OF LAWTON ENGINEERING DIVISION</b>		<b>PROJECT #: Engineering Update March 2012</b>	
		<b>DATE:</b>	
<b>FILE NAME: 12 Sidewalk And Driveway.dwg</b>			
DESIGNED BY: <b>G. Hennessee</b>	DRAWN BY: <b>S. Mallicoat</b>	PLOT DATE: <b>03/22/2012</b>	AS BUILT DATE: <b>SHEET OF</b>