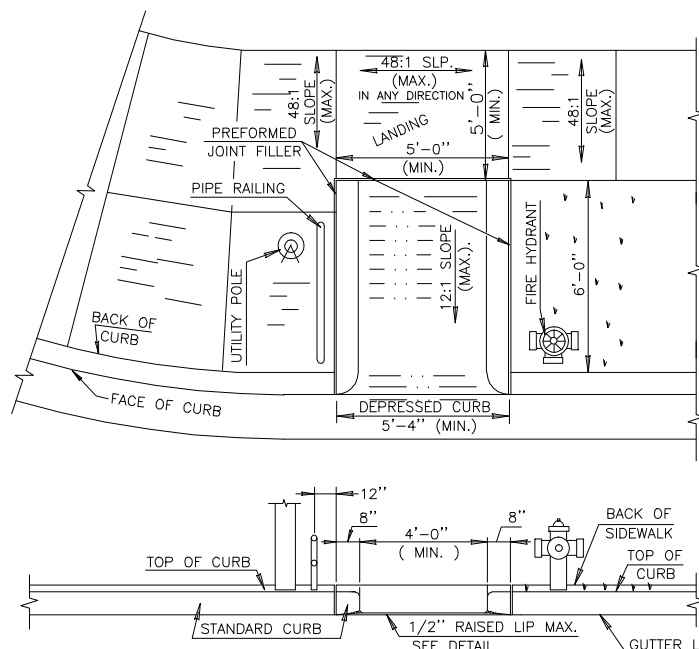
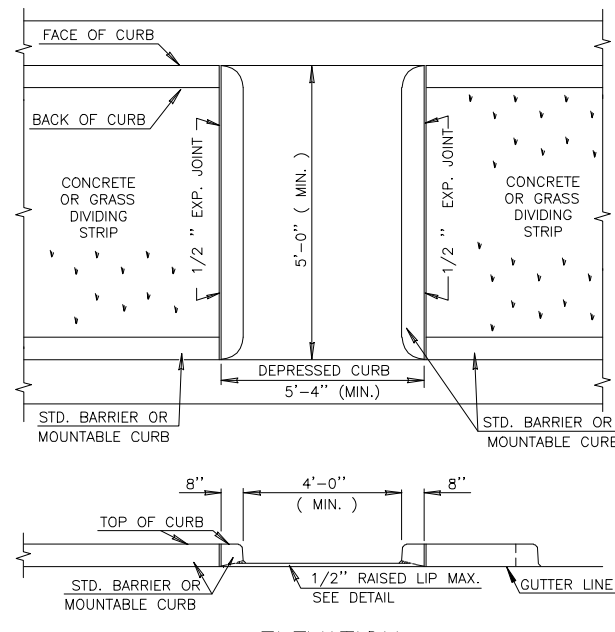


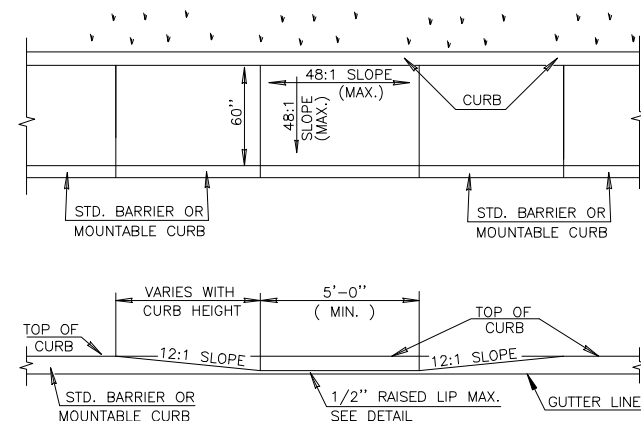
ELEVATION



ELEVATION



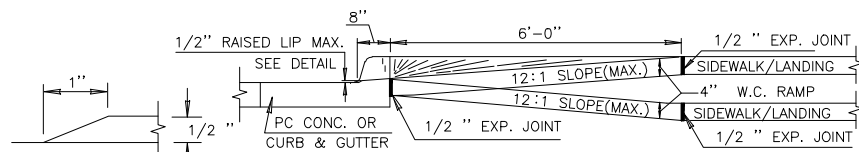
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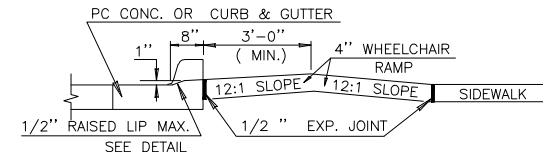
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GENERAL NOTES

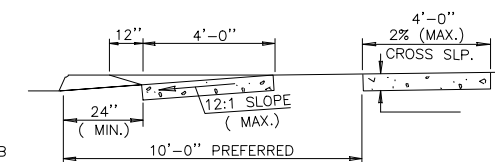
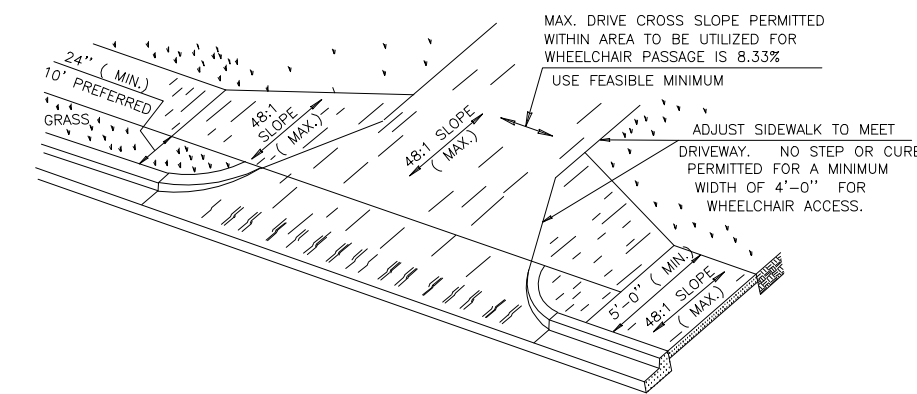
1. THERE WILL BE NO SEPARATE PAYMENT FOR RAMP AND TRUNCATED DOME CONSTRUCTION, AS ALL COSTS ASSOCIATED WITH THE WORK SHALL BE INCLUDED IN THE PRICE BID FOR "CONCRETE SIDEWALKS". THERE WILL BE NO DEDUCTION OF PAYMENT FOR CONCRETE CURB & GUTTER AND/OR INTEGRAL CURB FOR THE LENGTH OF THE DEPRESSED CURB.
2. RAMP AND LANDING WIDTHS SHALL MATCH SIDEWALK WIDTHS AND SHALL BE 4' MIN. UNLESS AUTHORIZED OTHERWISE BY THE ENGINEER.
3. LANDING LENGTHS SHALL BE 5' MIN. IF RAMP CHANGES DIRECTION, LANDING SHALL BE A MIN. OF 5'x 5'.
4. RAMP DIMENSIONS SHOWN ARE BASED ON A CURB HEIGHT OF SIX INCHES. THE DIMENSIONS SHOULD BE ADJUSTED FOR OTHER CURB HEIGHTS. THE MAXIMUM PERMISSIBLE SLOPES OF THE WHEELCHAIR RAMPS IS 8.33% (12:1). RAMP SLOPE MAY BE 10:1 (MAX.) ALONG FACE OF TAPERED CURB.
5. TRANSVERSE GROOVING (TINING) OR A MEDIUM BROOM FINISH SHALL BE REQUIRED ON RAMPS AS SHOWN ON THIS SHEET.
6. DRAINAGE STRUCTURES SHALL NOT BE PLACED IN LINE WITH THE RAMPS.
7. THE NORMAL GUTTER LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP WITH A 50:1 SLOPE (MAX.), SEE NOTE NO. 10.
8. WHEELCHAIR RAMPS SHOULD BE LOCATED SO THAT THE RAMP WILL BE ON THE TRAFFIC APPROACH SIDE OF ANY OBSTACLE.
9. WHEELCHAIR RAMPS SHOULD BE BUILT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE TYPE OF RAMP WILL BE DESIGNATED ON THE PLANS. IF A RAMP IS TO BE CONSTRUCTED AS A COMBINATION OF TWO TYPES, (ONE SIDE TYPE A AND ONE SIDE TYPE B) THE RAMP SHALL BE DESIGNATED AS TYPE A-B.
10. PIPE RAILING CONSTRUCTION DETAILS, WHEN REQUIRED AT TYPE B WHEEL CHAIR RAMPS, WILL BE SHOWN ON THE PLANS.
11. EXCAVATION, BACKFILL, EXPANSION JOINT MATERIAL, SEALERS, AND OTHER RELATED MISCELLANEOUS ITEMS WILL NOT BE PAID FOR SEPARATELY BUT THE COST THEREOF SHALL BE INCLUDED IN THE COST OF THE SIDEWALK.
12. ALL FEATURES OF CONSTRUCTION INCLUDING, BUT NOT LIMITED TO, SIDEWALKS, CURB RAMPS AND CROSSWALK MARKINGS SHALL COMPLY WITH THE AMERICANS WITH DISABILITIES ACT, ACCESSIBILITY GUIDELINES (ADAAG). WHERE SPATIAL LIMITATIONS OR EXISTING FEATURES WITHIN THE LIMITS OF THE PROJECT PREVENT FULL COMPLIANCE WITH THE ADAAG, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER UPON DISCOVERY OF SUCH FEATURE(S). THE CONTRACTOR SHALL NOT PROCEED WITH ANY ASPECT OF THE WORK WHICH IS NOT IN FULL COMPLIANCE WITH THE ADAAG WITHOUT PRIOR WRITTEN APPROVAL FROM THE ENGINEER. ANY WORK WHICH IS NOT PERFORMED WITHIN THE GUIDELINES OF THE ADAAG, FOR WHICH THE CONTRACTOR DOES NOT HAVE WRITTEN APPROVAL, SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE.



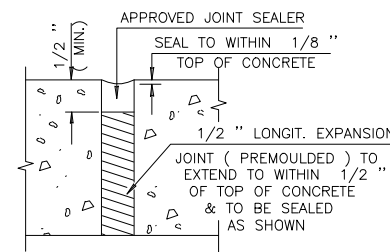
WHEELCHAIR RAMP PROFILES



WHEELCHAIR RAMP PROFILES

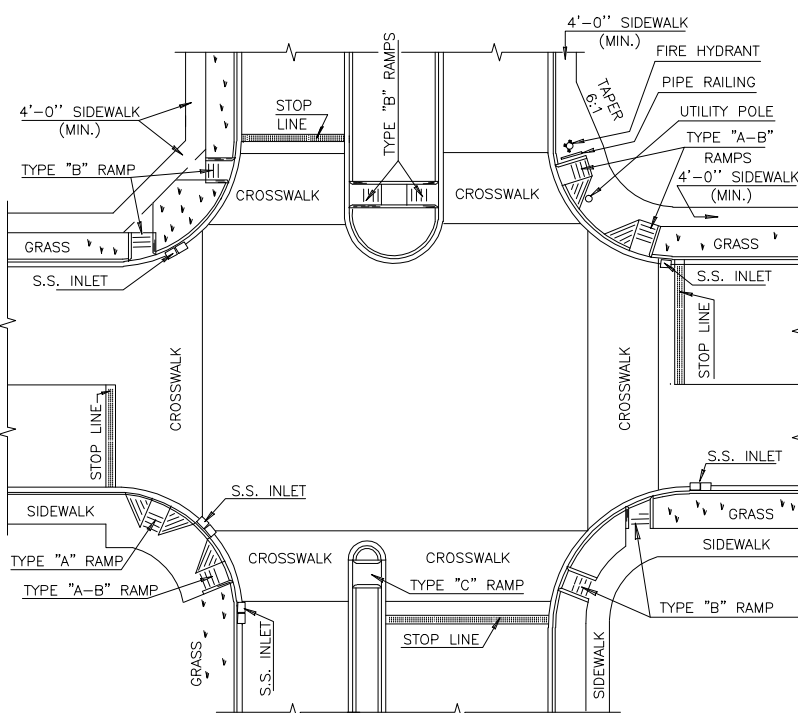


▲ AT DRIVEWAY LOCATIONS, THE NORMAL 4'-0" SIDEWALK MUST BE SET BACK FROM CURB, OR A WIDER SIDEWALK (5'-0" TYP.) USED TO ASSURE WHEELCHAIR PASSAGE. WHEELCHAIR PASSAGE MUST BE AVAILABLE ON EXTENDED LENGTHS OF SIDEWALKS AT INTERVALS LESS THAN 200 FEET.



1/2" EXPANSION JOINT

▲ JOINT FILLER SHALL MEET THE REQUIREMENTS OF SECTIONS 701.08(b) AND 701.08(e) OF THE ODOT 1999 ENGLISH STANDARD SPECIFICATIONS. JOINT FILLER SHALL BE USED BETWEEN SIDEWALK AND CURBS, WHEELCHAIR RAMPS, DRIVEWAYS, STREETS, RETAINING WALLS, ETC.



▲ THE ABOVE EXAMPLES ARE TYPICAL ONLY AND ARE SHOWN TO ILLUSTRATE POSSIBLE RAMP TYPES, POSSIBLE RAMP LOCATIONS, POSSIBLE INLET LOCATIONS, AND HOW THE RAMP WILL BE DESIGNATED ON THE PLANS. CARE SHOULD BE EXERCISED TO ASSURE THAT MEDIAN RAMP AND CURB RAMP LINE UP, AND THAT RAMPS THROUGHOUT A PROJECT ARE LOCATED WITH SOME DEGREE OF UNIFORMITY.

**STANDARD DETAILS
WHEELCHAIR RAMPS**

**CITY OF LAWTON
ENGINEERING DIVISION**

PROJECT NO.:	DATE:
ENGINEER UPDATE	DATE: APRIL 2007
ENGINEER UPDATE	MARCH 2011
ENGINEER UPDATE	MAY 2011
ENGINEER UPDATE	MARCH 2012

DESIGNED BY: G. Hennessy DRAWN BY: S. Malcom AS BUILT DATE: SHEET OF